



Orchard Road/Blackberry Creek Corridor Plan

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Village of Montgomery
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Job# 25364722



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Introduction

This report builds upon the preliminary findings of the previous submittal and presents Steps 4, 5, and 6 of the Planning Process for the Orchard Road/Blackberry Creek Corridor Plan. The document has been prepared by URS • TPAP (the Consultant), with significant advice, support and assistance from the Planning Advisory Committee, property and business owners, and others familiar and concerned with growth and development within the Corridor.

Overview of the Project

The Orchard Road/Blackberry Creek Corridor (the Corridor) encompasses an area approximately 4 square miles. Although much of the area is within incorporated Village of Montgomery, some areas are unincorporated Kane County and unincorporated Kendall County. The Corridor can generally be defined as a ½ mile area on the east and west sides of Orchard Road from Jericho Road on the north to the Village limits on the south.

The Corridor occupies a strategic location within the quickly growing western suburbs of Chicago. Situated on the boundary of two of the fastest growing counties in the region, the Corridor will likely experience significant pressure for growth and development. The 2002 Comprehensive Plan of the Village of Montgomery identifies several land uses for the Corridor, including residential, manufacturing/production, warehouse/ distribution, retail, and retail/residential mixed use.

This area is recognized as a unique opportunity along the rapidly developing western suburbs and Orchard Road Corridor. Tremendous growth is being experienced throughout Kane and Kendall County, and recent growth has occurred along Orchard Road to the north and south of the Village of Montgomery.

The uniqueness of the Corridor with Blackberry Creek offers an opportunity for creative planning concepts and techniques. Such an opportunity is unique in the history and future of the Village and requires extraordinary efforts to guarantee a high quality development in harmony with its surrounding area.

The Planning Process

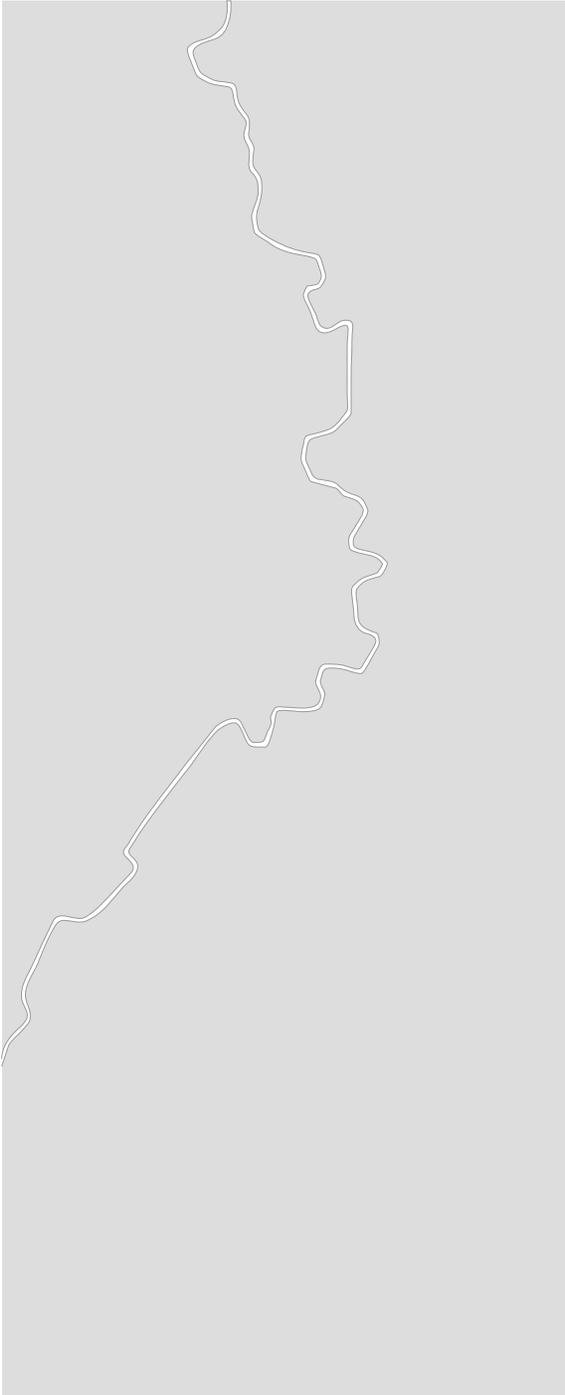
The Orchard Road/Blackberry Creek Corridor Plan (the Plan) will entail a seven-step planning process designed to assist the Village of Montgomery in formulating long-range improvement and redevelopment recommendations for the Corridor. The steps of the planning process are highlighted below.

Step 1: Project Initiation included a project initiation meeting to set the foundation for the planning process. The meeting included a project initiation workshop, designed to solicit the Planning Advisory Committee's (PAC) concerns and aspirations for the Corridor.

Step 2: Community Outreach included a workshop that was held to solicit general public input. Key person interviews were also conducted to obtain additional information from key stakeholders within the corridor.

Step 3: Data Collection and Analysis included a collection, review and analysis of all existing conditions, including past plans and studies, regional strengths and weaknesses, demographics, market and development potentials, existing lands use, current zoning, transportation and circulation, environmental conditions. Findings were presented in the Phase I Synthesis Report.

Step 4: Establishing a Vision utilized visioning workshops and allowed residents, business persons, and stakeholders to help shape a vision for the Corridor. The vision was also based on findings and feedback from earlier phases of the assignment.



Step 5: Goals and Objectives were established for the different land uses and areas of concern. In addition, this section included the creation of draft plans for land uses, conservation, transportation and community facilities.

Step 6: Design and Development included preliminary design and development guidelines, a marketing strategy, and a preliminary implementation plan.

Step 7: Corridor Plan was the preparation and review of the draft and final versions of the Orchard Road/Blackberry Creek Corridor Plan.

The planning process that was used throughout the development of the Orchard Road/Blackberry Creek Corridor Plan entailed significant public and private cooperation and participation. A number of opportunities existed throughout the entire planning process for property owners; merchants and business owners; property managers; community leaders; builders and developers; and Village staff and public officials to provide input and voice their opinions and concerns. In addition, a Planning Advisory Committee comprised of public organizations and private property owners, has been created to work with the Consultant on a regular basis throughout the course of the study.

The planning process was structured to establish trust and cooperation among the various parties, and build support and consensus for the Plan one-step-at-a-time.

Corridor Vision

A Vision for the Orchard Road/Blackberry Creek Corridor has been established based on community input received throughout the planning process. Supported by a detailed conditions analysis and guided by the Planning Advisory Committee, the Vision provides the foundation for future planning and improvement efforts.

The Vision Statement is intended to be a retrospective that chronicles the accomplishments and achievements that have been undertaken in the Corridor since the Orchard Road/Blackberry Creek Corridor Plan was completed in 2004.

The Vision Statement provides important focus and direction for the preparation of goals, objectives, policies, and recommendations of this Plan.

The Vision Statement is intended to provide a brief, overall "snapshot" of the Orchard Road/Blackberry Creek Corridor in the Year 2014. While it incorporates the main ideas and recurring themes expressed by residents and Village staff, it does not include all of the specific issues and areas of concerns suggested by participants.

Since Montgomery's Orchard Road and Blackberry Creek Corridor Plan was completed 10 years ago, the corridor has experienced significant economic and environmental investment. The Village's tax base has been strengthened and diversified using the environmental setting and public improvements as a catalyst for economic growth and development within the corridor.

In the year 2014 . . .

The improvements of Orchard Road, including widening, streetscape improvements, sidewalks, turn lanes and signalizations were successful, largely due to the cooperative partnerships between the Village of Montgomery, Kane County and Kendall County. Financed through the road improvement impact fees of both Counties, the Orchard Road improvements have addressed vehicular and pedestrian traffic, safety and access concerns. The intersection of U.S. Route 30 and Orchard Road, which is now safer and more efficient in terms of traffic flow, serves as a constant reminder of the successful cooperative effort between the three organizations.

Orchard Road has improved access to properties along the corridor and spurred industrial and commercial development. The 5-lane road has been completed for a number of years and still effectively and efficiently moves traffic throughout and within the corridor. Properly located access points coordinated and synched through comput-

erized traffic signals manage the efficient flow of traffic while at the same time provide for safe access to major roads and entrances within the corridor. The improvement in traffic management, the proximity of major transportation arteries such as Route 30, I-88, and the new Prairie Parkway, and the presence of major industrial companies such as Dial, Caterpillar and United Facilities have attracted additional businesses to the area. As a result, once undeveloped industrial sites along Cornell Avenue and Rochester Drive are now fully developed and contributing positively to the activity and sustainability of the corridor.

A wide range of retail opportunities have been developed within the corridor to serve the flourishing residential markets and the increasing number of people that call Montgomery "home." Several restaurants and convenience retail centers have been constructed on the west side of Orchard Road near the Aucutt Road, United Drive and Rochester Drive intersections. The commercial areas are anchored by grocery and larger national retail tenants. The businesses provide much needed shopping opportunities to Montgomery residents and local employees, strengthen the Village's tax-base, and capture retail sales tax dollars.



Strengthening the non-residential tax-base has not come at the expense of the corridor's appearance or density. The Plan's design guidelines and development recommendations for the commercial and industrial areas within the corridor have reinforced the small-town character of the Village of Montgomery and have ensured that new development is attractive and appropriately scaled.

The appearance of existing development has also improved through the implementation of façade improvement programs and an increased pride by property owners. A number of property owners have dramatically improved their properties, in a similar fashion to the Orchard Road Animal Hospital's improvements when it expanded over ten years ago. In fact the guidelines have directed the appearance so effectively that they have set the standard for development throughout the Village.

Other signage improvements and pedestrian amenities including park benches, street lights and banners, and right-of-way trees distinguish the corridor, and its development, from adjacent communities. The street lights provide both for the automobile and the pedestrian, and consist of soft, low-level lighting that creates a warm and welcoming atmosphere. Additionally, gateway signs and entry features have been installed; noti-

fying visitors they are entering the proud community of Montgomery.

Building on the Blackberry Creek Watershed Alternative Future's Analysis, the Orchard Road and Blackberry Creek Corridor Plan's implementation has adhered to the principals of conservation design. Conservation techniques were successfully employed to minimize negative impacts to hydrology and water quality, and the sites within the corridor have set the standard for environmentally sensitive design and development.

The effects of this sensitive development are most noticeably seen in Blackberry Creek, its banks, and the adjacent wetlands. The conservation design techniques utilized have protected these natural resources from runoff, thereby reducing the amount of sediments entering the creek. Water quality has noticeably improved and wildlife residing in and along Blackberry Creek is thriving. The development regulations and recommendations within the Plan have restored and enhanced Blackberry Creek and through a cooperative agreement between the Village of Montgomery, the Fox Valley Park District, and the Kane and Kendall County Forest Preserves, this natural resource serves as an educational classroom and recreational amenity for the community.



Sidewalks and paths along both sides of Orchard Road link to trails flanking Blackberry Creek and provide pedestrian and bicycle connections to Jericho Lake. The Aucutt Road bridge over Blackberry Creek has also been designed to accommodate pedestrians, providing an important east-west linkage. New trails have been constructed allowing safe passage from the corridor to the Stuart Sport Complex as well as to regional trails, such as the Virgil Gilman and the Fox River Trails. In addition, the trails link the residential areas to the new retail establishments affording residents an alternative to driving. Restaurants have taken advantage of the thriving natural setting by providing outdoor seating and patios overlooking the creek. Additionally, signage, benches, and gathering spaces provide an inviting and user-friendly experience.

Through the collaborative effort between the Fox Valley Park District and the Village of Montgomery, the extension of Aucutt Road was realized in a timely and cost-efficient manner without compromising the Fox Valley Park District's park site. This major collector has been instrumental in completing the "grid-like" street pattern within the community to efficiently move traffic throughout the entire Village. The Aucutt Road extension has also provided the transportation network necessary to adequately serve all areas of the Park District's sports complex, particularly during peri-

ods of intense uses such as tournaments. This win-win scenario for the Village and the Park District highlights the cooperative intergovernmental environment created by the Village and the Park District for the betterment of the community as a whole.

Goals and Objectives

The Orchard Road Blackberry/Creek Corridor Plan is an expression of what a community wants the corridor to become in the future. The plan, if carefully crafted, can become a reliable policy guide for decision-making and Village action.

The Orchard Road Blackberry/Creek Corridor Plan includes statements of goals and objectives, which provide the framework for planning recommendations, policies and future projects and actions:

Goals describe desired end situations toward which planning efforts should be directed. They are broad and long-range. They represent an end to be sought, although they may never actually be fully attained.

Objectives describe more specific actions that should be undertaken in order to advance toward the overall goals. They provide more precise and measurable guidelines for planning action.

Together, the goals and objectives paint a picture of what a community wants to accomplish with its Orchard Road Blackberry/Creek Corridor Plan, and with other development tools and ordinances. They provide direction, and serve as a guide for evaluating specific projects and land use alternatives.

The goals and objectives presented are based on: a) input from Village staff and the Project Advisory Committee; b) the community workshop; and, c) the key person interviews.

The goals and objectives are presented on the following pages.

Residential Neighborhoods

Goal:

Maintain and enhance the existing single-family residential areas within the Corridor, and continue to expand the housing inventory to support the current population; accommodate projected growth; and create a high-quality residential environment.

Objectives:

1. Preserve the open space character and natural features of Blackberry Creek and its floodplain, by encouraging conservation design and clustered single-family residential development on areas identified as Cluster Residential in the Land Use Plan.
2. Extend desirable characteristics of Lakewood Creek, including density, into Single Family Residential areas as presented in the Land Use Plan.
3. Provide a variety of housing types within the Corridor, including varying lot sizes and prices, to meet the needs of all existing and future residents of the Village of Montgomery.
4. Discourage monotonous appearance of new residential development by requiring architectural review. An anti-monotony code should be considered by the Village.
5. Protect residential areas from incompatible uses and collector streets by requiring buffering, berming, increased setbacks, and restricted access.
6. Preserve and enhance the existing housing through effective code enforcement and preventive maintenance.
7. Require adequate, usable open spaces in all new residential developments, designed to meet the needs of residents. All open spaces should be accessible by sidewalks, bike-ways, and trails.
8. Require developers to provide paved pedestrian linkages and sidewalks to daily activity areas such as the proposed trail along Blackberry Creek, all existing and future parks, the Municipal Civic Complex and other civic uses within the Village of Montgomery.
9. Require developers to provide for the connection or extension of planned streets to adjacent, undeveloped properties, to create transportation linkages between residential neighborhoods and to provide street system continuity.

Commercial Development

Goal:

Promote Corridor Commercial and Neighborhood Commercial development at appropriate locations within the Corridor that will provide local residents and the surrounding region with a diversity of goods and services, enhance the Village's tax base, improve the appearance and character of the corridor, and provide opportunities for employment.

Objectives:

1. Require conservation design and development for commercial development within the Corridor north of Baseline Road where it is adjacent to Blackberry Creek. Conservation design should also be promoted and encouraged in other commercial areas. Conservation design differs from conventional design in that it utilizes site planning and design to preserve existing and natural areas and utilizes naturalized drainage and detention measures to minimize negative impacts to hydrology and water quality.
2. Encourage the development of a variety of new commercial uses that will increase the range of shopping and employment opportunities, and strengthen and expand the Village's tax base.
3. Market the Corridor Commercial areas as presented in the Land Use Plan, as major commercial centers within the Village of Montgomery and surrounding area that could include both local and regional-scale shopping centers, restaurants and department or discount stores.
4. Promote the development of Neighborhood Commercial, to provide for the convenient shopping needs of Montgomery, as indicated in the Land Use Plan.
5. Encourage the enhancement and improvement of existing commercial uses within the Corridor by implementing a façade improvement program, and encouraging improved site design, landscaping, architecture and signage.
6. Require all commercial development to conform to the Design Guidelines presented in this Plan.
7. Require developers to provide sidewalks within all commercial development. Sidewalks should be located along both sides of all streets, and internally within each site to provide for pedestrian access to shopping areas, and promote internal pedestrian linkages in commercial developments.
8. Minimize and mitigate any negative impacts of commercial and business uses activities on neighboring land-use areas.
9. Encourage the corrective maintenance and rehabilitation of older commercial and business properties that are in poor condition or do not conform to community standards.

Business Park and Industrial Development

Goal:

Continue to develop business park and light industrial uses to ensure a diversified economy for the Village; strengthen the Village's tax base; and provide employment opportunities for Village residents.

Objectives:

1. Market the Business Park areas, as presented in the Land Use Plan, for distribution, logistics, high-tech, office, and research development.
2. Market the Industrial areas, as presented in the Land Use Plan, for light and heavy manufacturing uses, similar and compatible to the existing Caterpillar site.
3. Direct non-polluting light industrial, warehouse and distribution and heavy commercial land uses to areas designated as Business Park.
4. Promote the development of vacant sites, and underutilized sites within existing business areas, specifically the vacant sites along Rochester Drive and Cornell Avenue.
5. Business park uses should include and accommodate large-scale office developments.
6. Require all business park and industrial development to conform to the Design Guidelines presented in this Plan.
7. A limited amount of convenience commercial development, designed to serve the day-to-day needs of business park and industrial employees and patrons, should be considered within business park and industrial developments at strategic locations. Earth Mover Credit Union is an example of an appropriate convenience commercial use in an industrial area.
8. Access to individual building sites within business parks and industrial area should be from local streets within the Corridor. Access from Orchard Road, or collector streets should be limited to major entrances serving multiple businesses.

Transportation, Circulation and Access

Goal:

Provide a transportation system consisting of streets, sidewalks, trails, and public transportation services, to provide for the safe and efficient movement of vehicles, bicycles and pedestrians. The transportation network should enhance Blackberry Creek where possible, and reinforce the proposed Land Use Plan and adjacent development patterns.

Objectives:

1. Maximize safety and maintain the operational efficiency of Orchard Road by requiring the consolidation of curb cuts.
2. Maximize safety and maintain the operational efficiency of Orchard Road by minimizing the number of curb cuts allowed for new development.
3. Require cross-access between adjacent commercial properties, and consolidate access drives to improve safety and maintain the operational efficiency of traffic flow on Orchard Road.
4. Prohibit access from Brentwood Avenue onto Orchard Road to improve the safety of residents entering and exiting the residential area northeast of the intersection of Baseline Road and Orchard Road.
5. Support the implementation of the Village's Transportation Plan for the identification of where new roads and access points are needed to serve projected growth, so that adequate rights-of-way can be reserved as development occurs.
6. Provide signalized access at intersections identified in the Transportation Plan when warranted by necessary traffic counts.
7. Ensure that future roadway infrastructure, throughout the Corridor and adjacent areas, provides for multiple east-west and north-south roads to mitigate the potential of congestion and over use of select roads.
8. Realign the intersection of Baseline Road and U.S. Route 30, Kevin Road, and other problematic intersections, to eliminate dangerous vehicular conflicts caused by poorly placed and designed intersections.
9. Expand the pedestrian circulation system by requiring all developers to provide sidewalks along both sides of all streets.
10. Develop a comprehensive network of multi-use trails, utilizing a recreational trail along Blackberry Creek as a backbone, linking residential areas, shopping areas, and parks and open spaces.

The Natural Environment

Goal:

Preserve, protect and enhance Blackberry Creek, and other natural resources and environmentally sensitive areas that may be compromised by development within the Orchard Road/Blackberry Creek Corridor.

Objectives:

1. Require all developers to preserve, enhance, and protect the natural environment, including Blackberry Creek, floodplains, wetlands, wildlife habitat, waterways, topography, and forested areas.
2. Protect and preserve Blackberry Creek and its floodplain, by maintaining them as permanent open spaces.
3. Prioritize the acquisition of, or the establishments of easements on, lands determined to be necessary to protect Blackberry Creek from development.
4. Promote best management practices for stormwater, including green roofs, bioswales, naturalized detention areas, porous pavement, rain barrels and cisterns, rainwater garters, filter strips and level spreaders, and vegetated swales to ensure that new developments do not contribute runoff that increases off-site flooding or degrades water quality in surface or ground water systems.
5. Develop and adopt best management practices for landscaping, requiring installation of native plant species to serve a variety of purposes including habitation, infiltration, filtering and evapotranspiration.
6. Discourage development in areas where unfavorable soil conditions exist, such as hydric soils that are saturated, flooded or ponded for at least part of the year.
7. Preserve existing, mature trees, within the Corridor by maintaining and enforcing the Village's tree preservation ordinance and through working with developers to identify desirable tree species, and alter development proposals to avoid impacts to these trees.
8. Protect aquifers, wetlands and streams from pollutants by promoting reduction in the amount of eroded soil, fertilizers, herbicides, insecticides, and other physical, chemical or biological substance carried into waterways by stormwater runoff.

Community Facilities and Infrastructure

Goal:

Provide necessary public facilities and infrastructure, including police and fire protection, telecommunications, water mains, and sewer mains, for all anticipated development within the Orchard Road/Blackberry Creek Corridor.

Objectives:

1. Support the development of the Montgomery Civic Center Campus to provide a high level of police and other civic services to the Corridor and the Village as a whole.
2. Support the development of a fire station at the proposed location along Galena Road to provide quick response time to development within the Corridor.
3. Encourage efficient site design and land planning to minimize inefficiencies and reduce costs of providing public services, such as: snow plowing and road repair; postal service; emergency services (fire, police and ambulance); and school bus travel.
4. Continue to provide adequate water distribution, sanitary sewer and storm sewer systems within the Corridor.
5. Ensure that all public sites and buildings are attractive and well-maintained, and that they complement the areas in which they are located.
6. Accommodate the development of telecommunications within the Corridor, including telephone, cable and high-speed internet.

Parks, Recreation and Open Space

Goal:

Promote adequate, well-designed parks, open spaces and recreational facilities designed to serve the needs of all segments of Montgomery's population.

Objectives:

1. Cooperate with the Fox Valley Park District, Kane County Forest Preserve District, and Kendall County Forest Preserve District to ensure Blackberry Creek and its sensitive floodplains are protected from development and enhanced as community amenities.
2. Require developers to provide usable, centrally located parks and recreational facilities within residential areas.
3. Cooperate with the Fox Valley Park District to accept conveyance of, develop, program, and maintain new neighborhood parks within the residential areas of the Corridor.
4. Continue to support the development of Stuart Sports Complex and provide necessary access points and roadways to accommodate the park's peak periods.
5. Develop a multipurpose trail system, utilizing a recreational trail along Blackberry Creek as a backbone, linking residential areas, shopping areas, and parks and open spaces. Existing rights-of-way, protected greenbelts, open space corridors, and easements should be utilized to provide connections between subdivisions, parks, shopping areas, public facilities, open spaces, and other multi-use trails in the area.
6. Require developers to equip parks in the first phase of their residential subdivisions, to ensure that commitments for open space and recreation are maintained, and provided in a timely fashion.
7. Ensure that all parks and open spaces are adequately and attractively maintained and that reforestation is undertaken as required.
8. Ensure safe and convenient pedestrian and bicycle access to all park sites and recreational areas.

Corridor Appearance & Identity

Goal:

Establish the Orchard Road/Blackberry Creek Corridor as an attractive and appealing corridor that is distinguishable from other business corridors within the region through gateway signage, landscaping, maintenance code enforcement, and Village-sponsored beautification programs.

Objectives:

1. Establish the image and identity of the Village throughout the Corridor by creating well defined entrances to the Corridor and the Village of Montgomery.
2. As Montgomery continues to grow and develop, strive to maintain the Village's small-town character and traditional heritage, while still accommodating new development.
3. Create well-defined entrances into the Village through special treatments of signage, landscaping, and other design techniques, including landscaped parkways and building setbacks, and ornamental lighting.
4. Ensure architectural compatibility between commercial developments, and provide consistency in landscaping, setbacks, lighting and signage.
5. Encourage local groups and organizations to participate in the beautification of local parks and other public spaces through plantings of flowers, trees and shrubs.
6. Preserve, replace and plant trees and other vegetation in public spaces, including the public rights-of-way.

Physical Plans

- This section presents plans for the Orchard Road/Blackberry Creek Corridor. Included with the plans is a figure illustrating existing land-use. The plans illustrated in this section include:
- **Land Use Plan.** Provides land use designations for each parcel within the Corridor. Land use designations include Cluster Residential, Single-Family Residential, Commercial, Industrial, Business Park, Public, and Parks and Open Space.
- **Open Space, Recreation and Community Facilities Plan.** This plan illustrates all areas within the Corridor to be preserved as open space and for public use. Recreation amenities such as trail locations, including trail heads are included in this figure.
- **Transportation and Access Plan.** Presents proposed improvements and recommendations to improve access to all sites within the Corridor, while maintaining the safe and efficient flow of traffic.

Figure 1
Existing Land Use



The Orchard Road/Blackberry Creek Corridor includes a diverse mix of land-uses including residential, commercial, industrial/business park, and open space. Figure 1 illustrates existing land-use, based on field surveys undertaken by the Consultant in the Fall of 2003. The existing land-use pattern is an important consideration in assessing future improvement and development potentials within the Corridor.

Single-Family Residential. The single-family residential areas within the corridor consist of a mix of older residential developments, newer residential developments, and scattered single-lot homes. One of the newer residential developments within the Corridor is Lakewood Creek, located west of Orchard Road, north of Galena Road. The older residential development is located on the west side of Orchard Road, north of Route 30. The development consists of mix housing types and sizes, constructed on seemingly an ad-hoc basis. This existing development is a sharp contrast to newer residential development taking place in other areas of the community.

Multi-Family Residential. Multi-family uses within the corridor are located on the east side of Orchard Road. The uses consist of older apartment buildings located near Jericho Road and east of the single-family neighborhood north of Route 30.

Commercial. There are very few existing commercial uses within the Corridor. Orchard Road Animal Hospital and Earth Mover Credit Union are examples of some of the few existing commercial uses within the Corridor.

Industrial/Business Park. At the present time, industrial and business park uses represent most of the existing development within the corridor. Most of the east side of Orchard Road has been developed as industrial and business park uses. The uses vary in size and intensity. The larger and intense users along the corridor are distribution and logistic companies. United Facilities Inc. is the largest, with a 1 million square foot facility located east of the intersection of Orchard Road at United Drive. Hodgman and Superior Beverages are

other distribution companies that are more typical in size and intensity. Other industrial/business park uses within the Corridor include Dival and Caterpillar. These large manufacturing uses are located on the east side of the Corridor. While their properties do not front Orchard Road, the uses have impact on traffic and circulation within the area.

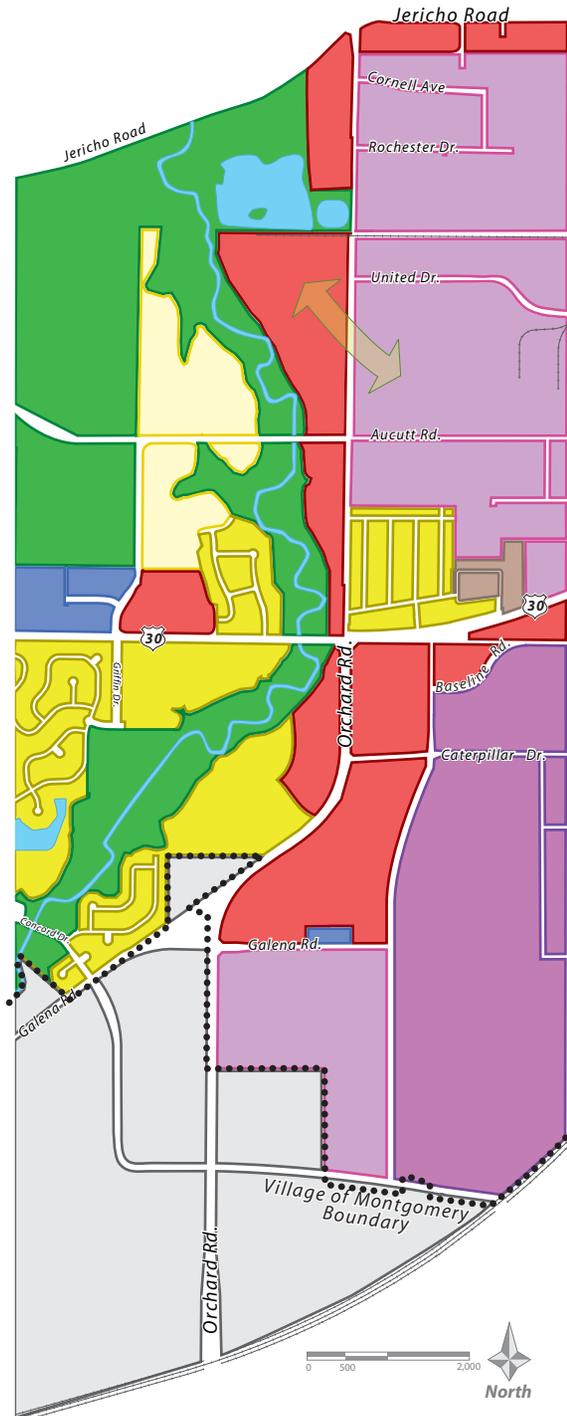
Parks and Open Space. All of the incorporated areas within the corridor are within the taxing boundaries of the Fox Valley Park District. There are 2 Park District sites within the corridor: Jericho Lake Park; and, Stuart Sports Complex. Jericho Lake Park is a passive community park that provides a picnic area, a shelter, a trail and water access to residents. Jericho Lake Park is accessible from Jericho Road. Stuart Sports Complex is an active recreation area that contains light ball fields, soccer fields and trails. Stuart Sports Complex is accessible from both Barnes Road and Jericho Road.

Vacant/Agricultural Land. Active farm land predominates most of the Corridor. While this land is currently farmed, growth and development pressure, combined with increasing costs of farming operations in the region, will almost certainly result in the development of the property for other uses in the near future. Agricultural land provides new development with a "clean slate" for planning and development.

Public. Two of the three sites illustrated as public are currently under construction. A police station is currently being developed on the north side of Baseline Road, west of Orchard, and a fire station is under construction southeast of the intersection of Orchard Road and Galena Road. Keck Memorial Cemetery is the other public use.

Floodplain. The 100-year floodplain encompasses a majority of the study area to the west, along Blackberry Creek. An overflow route also runs southeast under Orchard Road between approximately United Drive and Aucutt Road.

Figure 2
Land Use Plan



Residential. A market study indicated a strong market for large-scale residential development within the Corridor. The land use plan illustrates 3 different types of residential development. Each of the residential uses is described in more detail below.

Clustered Single-Family Residential. Clustered single-family residential consists of single family homes developed at the same gross density as typical residential developments within the Village of Montgomery, however the homes are clustered or grouped together to provide more open space. Clustering homes is the preferred development technique when protecting or providing open space is a community priority. Preservation of Blackberry Creek was identified as a priority by residents and stakeholders during the community outreach component of this assignment, and as a result, 102-acres of land west of Blackberry Creek are recommended for low density clustered single family residential development.

Single-Family Detached Residential. The single family detached residential areas should expand on the recent residential development that has occurred between Galena Road and U.S. Route 30, west of Orchard Road. New single-family detached residential should emulate the density, street patterns, sidewalks, landscaping and other appropriate design elements of the Lakewood Creek development. New single-residential development should be protected and buffered from incompatible uses and heavily trafficked roads including Orchard Road and U.S. 30. Homes should not front these road and the rear of the homes should be bermed and screened from the noise, light, dust, glare and other impacts from the adjacent nonresidential uses.

While the existing county subdivision at the northeast corner of U.S. 30 and Orchard Road is less than an ideal land use for the location, the subdivision is well established. Although a commercial development could take better advantage of the traffic and visibility, thus better expanding the Village's tax base, the Plan assumes this area will remain as residential use for the foreseeable future.

Multiple-Family Residential. The multiple-family residential illustrated on the Land Use Plan includes only existing multi-family developments. No additional multi-family dwellings are proposed as part of this Plan. Only the apartment complex east of the existing county subdivision remains. Multi-family and/or townhomes may be incorporated into larger single-family developments where appropriate.

Commercial. The commercial designation on the Land Use Plan is reflective of the market study, which indicated a strong market for commercial uses within the corridor. Commercial uses along Orchard Road should consist of a mixture of Convenience Commercial uses and Corridor Commercial uses. Convenience Commercial uses, intended to provide shopping for a localized population should be developed on the narrow areas along Orchard Road, and on all commercial property on Jericho Road as well as the parcel at the intersection of Griffin Drive and U.S. Route 30. The remainder will be Corridor Commercial, attracting shoppers from a larger area and consist primarily of destination retail, generating single-purpose automobile trips, although some service uses, such as the Orchard Road Animal Hospital, should be considered appropriate in some locations. While most of the properties will cater primarily to the automobile, the pedestrian should not be totally neglected. The Open Space, Recreation and Community Facilities Plan illustrates a trail and pathway network that will connect pedestrians to all properties within the corridor. Blackberry Creek, and its associated wetlands and floodplains should be incorporated into the overall design of commercial sites, and should be enhanced through substantial setbacks and buffering from environmental features, and the establishment of outdoor plazas and seating areas where appropriate.

Industrial and Business Park. Light industrial, business park, logistic and manufacturing uses predominate the east side of the Orchard Road Blackberry Creek Corridor. The Land Use Plan identifies 651 acres of land for industrial and business park use. Business park and industrial uses play a significant role

in the Village's identity and in the diversification of its tax base. A market study indicated a limited demand for industrial uses, and that a 100-acre industrial park could be absorbed within a 10-year time period. Industrial properties within the Corridor will benefit from the restricted, signalized, and coordinated access for United Drive, Rochester Drive and Aucutt Road proposed as part of the Transportation and Access Plan. Industrial development should be attractive and should conform to design guidelines of the Plan. Residential areas should be appropriately and sufficiently screened and buffered from any negative impacts from any adjacent industrial uses including noise, light, glare, dust, vibration, odor and traffic.

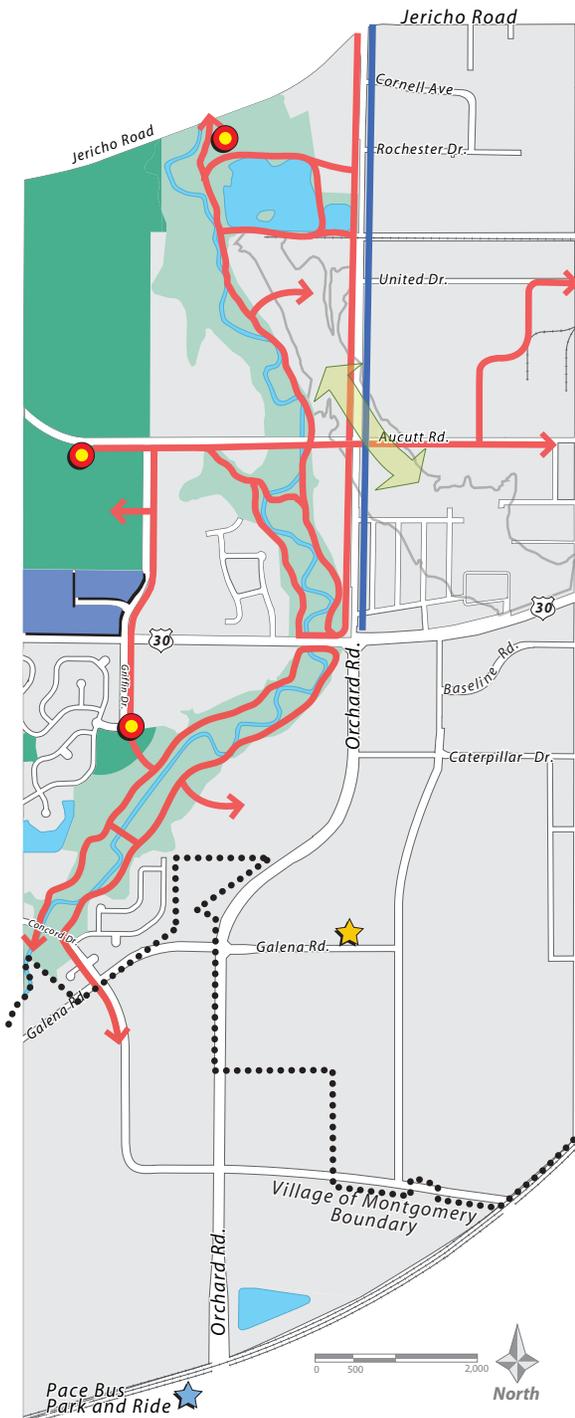
Public. There are 2 sites designated for public use on the Land Use Plan, both of which are currently under construction. A municipal campus is planned for a 29.2 acre site north of U.S. 30 west of Orchard Road, and a fire station has been constructed on Galena Road, east of Orchard Road. These facilities are covered in more detail in the Open Space, Recreation & Community Facilities Plan.

Parks and Open Space. Parks, open space, and greenways add significantly to an areas desirability and value. A greenway network, along with centrally located neighborhood parks are proposed in residential areas to complement a large greenway protecting and enhancing Blackberry Creek. Parks and open space are covered in more detail in the Open Space, Recreation & Community Facilities Plan.

Figure 3
Open Space, Recreation
and Community Facilities Plan



Montgomery Civic Center Campus
Master Site Plan



★ Fire Protection. A fire station has been constructed along Galena Road, east of Orchard Road. The land has been donated and the Oswego Fire Protection District will soon open the fire station. Although the fire station will provide necessary protection, particularly to the rapidly growing residential areas and the planned industrial areas, the location is in the midst of an area planned for single-family residential uses. Homes should be protected from the emergency vehicles, which should be encouraged to use Galena Road or the proposed collector street located east of the facility.

▬ Civic Center. The Village of Montgomery's Civic Center is currently under construction. The Civic Center will be home for the Village's police department, and the concept plan calls for 3 public buildings, 2 plazas, and a lake.

▬ Active Recreation. Active recreational areas are parks dedicated to providing for popular recreational activities, including ball fields and soccer fields. Stuart Sports Complex is the largest active recreation area, and once fully developed will contain 8 softball/baseball fields and 12 soccer/football fields. The facility will accommodate athletic leagues and tournaments and could be tapped as an economic development asset by commercial business that would choose to cater to the park's users. Access to the facility should be provided at 3 locations, to ensure the park is not congested at peak times. Access should be provided to the north at Jericho Road, the south at Baseline Road, and to the east by the proposed Aucutt Road extension.

▬ Passive Recreation. Passive recreational areas consist of open spaces designed to provide for unstructured recreational experiences, and environmental protection and education. A significant amount of land area is planned for passive recreation, and should be used to protect and enhance Blackberry Creek and its associated floodplains and wetlands. Installation of trails and interpretative signage will allow Montgomery residents of Montgomery to experience and interact with the natural areas and learn about their local natural resource and learn about its local and regional significance. The trail network should connect to all adjacent uses, including residential neighborhoods and commercial sites.

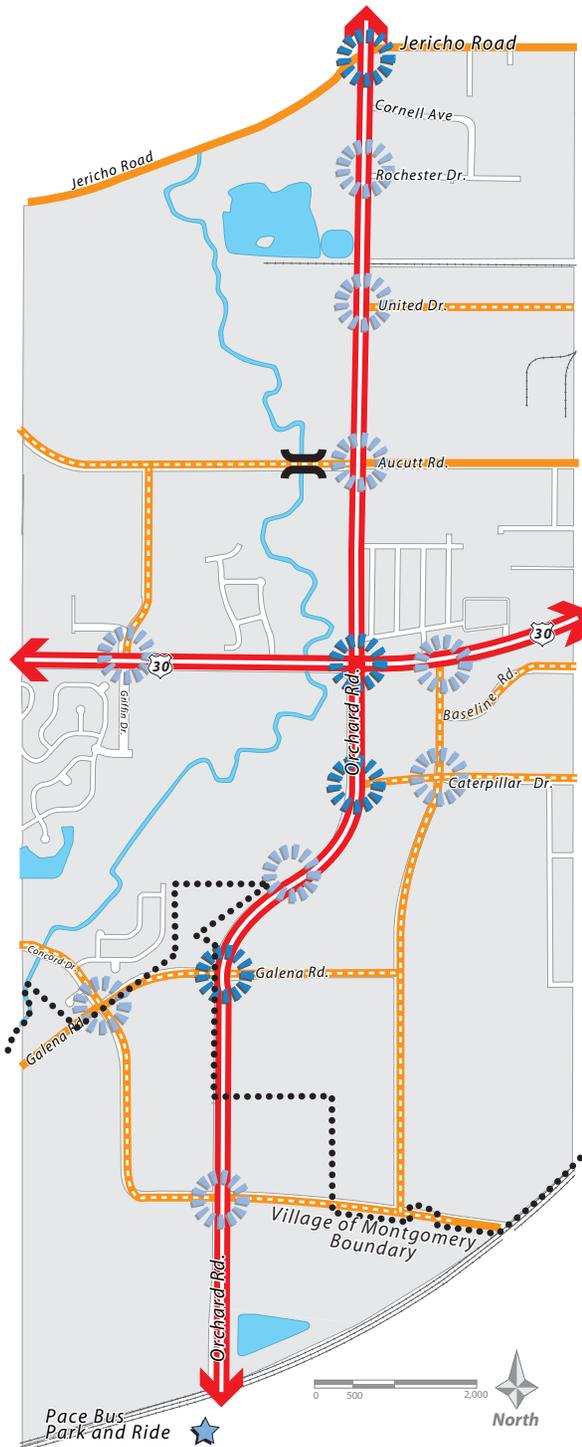
▬ Future Parks. As the Corridor grows to include almost 1,000 acres of residential land, the Village should use the parkland dedication ordinance to secure centrally located, high-and-dry neighborhood park sites in each residential unit, as formed by the collector and arterial street system on the Open Space, Recreation and Community Facilities Plan. Centrally located neighborhood parks ensure equitable access by all residents to park sites and will add significantly to the residential areas appeal, identify and desirability. Neighborhood parks should be connected to one another through an off-street greenway network (on-street may be necessary to cross Galena Road), and ultimately connect the parks and the neighborhoods to the greenway and trail network adjacent to Blackberry Creek.

▬ Trails. A crushed limestone or asphalt trail should be constructed on both sides of Blackberry Creek with a bridge connecting the two sides, and through new residential areas, connecting new neighborhoods to the natural amenity. The trail should be constructed to IDOT or Fox Valley Park District standards for the bike path designated along the west side of Orchard Road.

● Trailhead. Trailheads mark possible starting and ending points for users of the recreational trails. Trailheads could be developed to provide parking, signage, mile posts and trail information for trail users of the facilities.

▬ Sidewalks. Sidewalks should be constructed on both sides of all streets within the Corridor. IDOT's current plans for the widening of Orchard Road do not contain plans for sidewalks or a pathway on the east side. Regardless of IDOT's proposal, this plan recommends a sidewalk along the east side of Orchard Road.

↘ Overflow Route. A stormwater overflow route exists over Orchard Road. During significant rainfalls, when the culverts cannot handle the amount of stormwater, water travels along the overflow route and saturates the properties.



Arterials. Orchard Road and U.S. Route 30 are the two major arterial streets within the Corridor. Arterial streets are wider, faster, and have limited access points along their length. They also primarily carry thru-traffic whose origin and destination are outside of the Village of Montgomery. Commercial and non-local traffic should be directed to, and remain on, major arterial roads. Orchard Road averages 19,400 vehicles per day. U.S. Route 30 averages 15,200 vehicles per day.

Collector Streets. Collector streets are streets that collect traffic from local streets, and move the traffic toward arterial streets for efficient flow. Collector streets are intended to serve the local population, providing local streets with efficient access to arterial streets. Collector streets in the Corridor include Jericho Road and Aucutt Road.

Proposed Collector Streets. The Village of Montgomery's Transportation and Access Plan identifies several new collector streets within the Corridor. The most significant improvements are the extension of Aucutt Road west through the Stuart Sports Complex, a north-south collector between the Aucutt extension and the Police Station, and the realignment of Baseline Road.

Signalized Intersections. There are 4 signalized intersections in the Corridor. These traffic control systems allow for the safe and efficient movement of vehicles traveling within, along or through the Corridor.

Proposed Traffic Signals. A number of traffic signals have been proposed as part of the Village of Montgomery's Transportation Plan. The future signals are located at all collector/collector intersections, arterial/collector intersections, and arterial/arterial intersections. Specific details regarding the intersections, including turn lane designations are not specified in the plan.

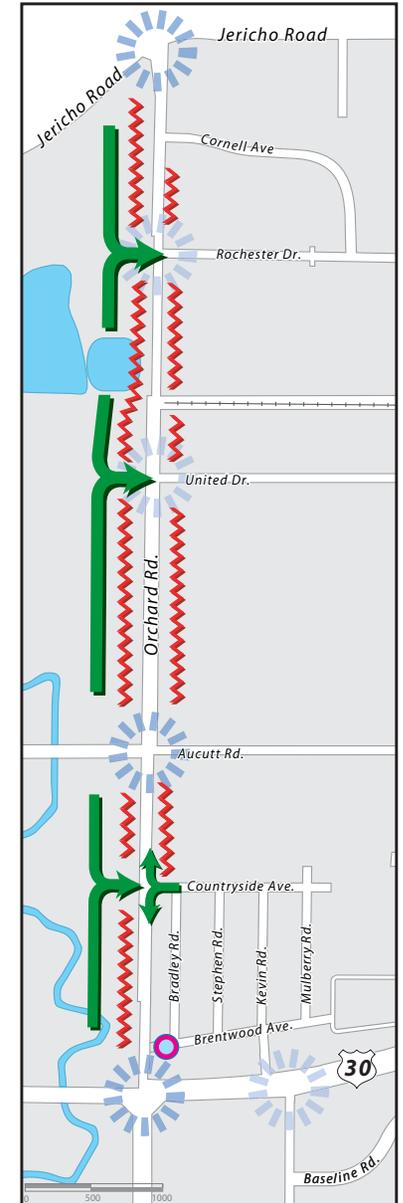
Figure 4 Transportation & Access Plan

Proposed Bridge. A bridge over Blackberry Creek will be required for the Aucutt Road extension. The bridge should take into consideration the proposed trail and trail heads traveling along both sides of Blackberry Creek.

Cul-de-sac. Restricting access onto Orchard Road from Brentwood Avenue will improve safety and efficiency of traffic flow. Full access to this residential area would still be provided at Countryside Avenue, and at a "jogged" signalized intersection at Kevin Road.

Restricted Access. A common concern by stakeholders and the public was the flow of traffic along Orchard Road and the safety of motorists. Restricting access along Orchard Road will achieve the desired community objective, by limiting left-hand turns to only a few key locations within the Corridor. Restricted access areas could still accommodate "right-in/right-out" turning movements, but should be determined by the Village and Kane County on a case-by-case basis.

Unified Access. Not every parcel and commercial business should be granted an exclusive access point or curb cut onto Orchard Road. Adjacent business and properties should connect parking areas and eliminate unnecessary access onto Orchard, creating a safer and more efficient auto corridor. Unified access should be restricted to the location of the existing and proposed traffic signals, including Rochester Drive, United Drive, and Aucutt Road, and at Countryside Avenue. Along Aucutt Road and U.S. 30, where the streets abut proposed commercial areas, access from these roads should be provided if it can be done safely.



Orchard Road Access Detail

Design and Development Guidelines

This section provides a more complete listing of design and development guidelines for the Orchard Road/Blackberry Creek Corridor. The guidelines address both the public and the private improvements within the Study Area, and are intended to support and strengthen land-use and development and public improvement recommendations found in this Plan. Design and development guidelines are divided into four categories:

General guidelines for all sites and buildings within the Corridor;

Guidelines applicable to commercial, industrial and business park buildings, sites, and development within the Corridor;

Guidelines applicable to residential buildings and sites within the Corridor; and

Guidelines applicable to proposed public improvements within the Corridor.

In general, the design guidelines strive to:

1. Promote public and private improvements and developments that will help create and reinforce the desired character and identity for the Orchard Road/Blackberry Creek Corridor.
2. Promote new development that complements the existing and desired scale and character of the Corridor.
3. Improve the appearance and quality of existing sites and buildings within the Corridor.
4. Ensure high-quality and compatible building and site design throughout the Corridor.
5. Establish a development pattern that enhances Blackberry Creek as a community amenity, and is sensitive to, and protective of, the natural environment.
6. Foster development that encourages more significant pedestrian and bicycle activity, while still accommodating automobile and truck traffic.

These guidelines do not attempt to dictate architectural styles or make all the buildings look the same. Rather, they strive to promote a level of quality, compatibility, and consistency that will help make the Corridor attractive and distinguishable from other surrounding areas. The guidelines will help the corridor become a unique and distinctive area for employees, shoppers, residents, and visitors alike.

General Guidelines

The guidelines for all sites and buildings, including both existing and anticipated development, are focused on promoting high-quality and compatible developments that will help create an attractive, distinguishable, and unified image and identity for the Orchard Road/Blackberry Creek Corridor. While the guidelines are specific enough to ensure design compatibility, they are also flexible enough to allow for individual creativity on the part of property owners, architects, and builders.

The guidelines presented below will be used by Village staff and the Planning and Zoning Commission in reviewing plans and proposals for all new developments within the Corridor. They should be considered “supplements” to the Village’s Zoning Ordinance and other applicable codes and ordinances.

Architects, property owners and developers should also use the guidelines as a reference as they prepare plans for new development projects.

To ensure the successful completion of these guidelines, letters of credit or security bonds should be required from developers. These funds would cover the cost of monitoring native grasses and plants until they establish.

Building Scale and Proportion

- Buildings may have either a pitched roof profile or a flat roof with a parapet wall. Parapets should be encouraged to create an interesting building profile and to hide HVAC equipment and other rooftop equipment. Mansard roofs should not be permitted along Orchard Road or within the Corridor.
- Exterior building design features that can help distinguish Orchard Road as it passes through Montgomery from other communities should be encouraged.
- Vertical architectural elements, such as a clock towers, spires or bell tower, should be considered as design “highlights” at key locations, such as the intersection of Orchard Road and Jericho Road and Orchard Road and U.S. Route 30.

Building Placement and Orientation

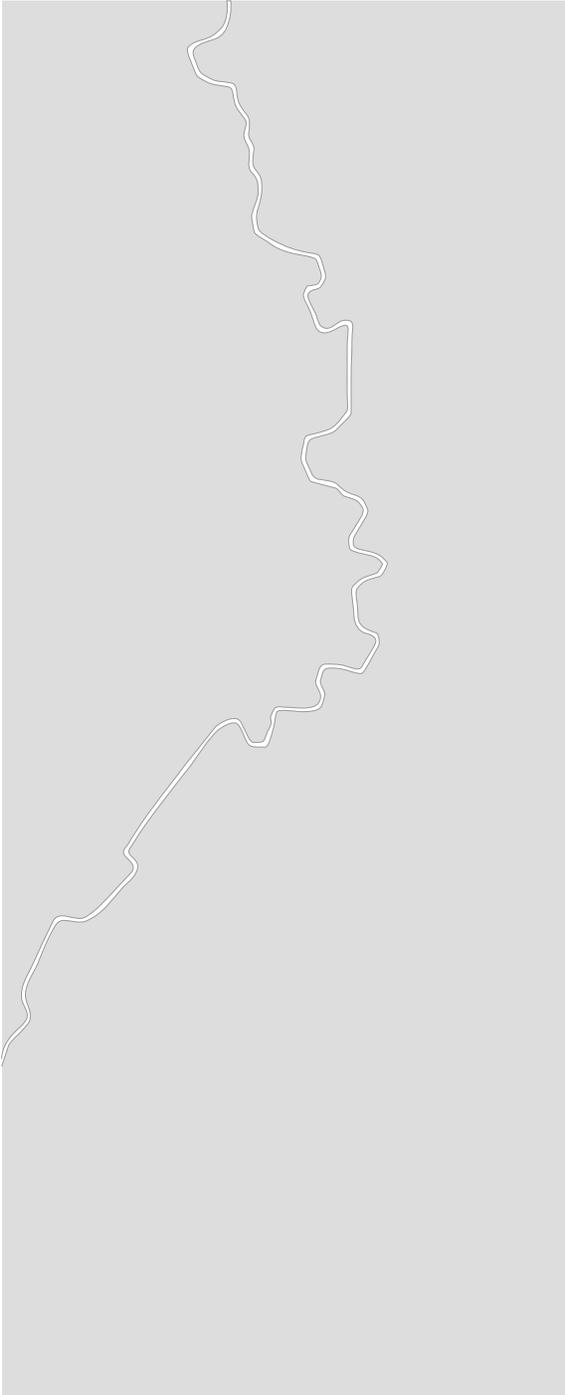
- Buildings should have a strong visual and physical relationship to Orchard Road to enhance the identity and pedestrian orientation of the Corridor. Buildings should be attractive at both a pedestrian and vehicular scale.
- The Orchard Road/Blackberry Creek Corridor will be characterized by building setbacks, side yards and surface parking lots. Consequently, the street frontage should be maintained through the use of low-profile landscaping and decorative walls or fencing.
- All buildings should “front” Orchard Road, (or Collector Streets if there is no Orchard Road frontage) wherever possible. When front doors face north or south, display windows or

distinctive facade treatments should be provided along public road frontage.

- Long, blank facades along Orchard Road should be avoided. More consideration should be given to industrial and business park uses along Orchard Road to help maintain a pedestrian oriented appearance.

Building Materials and Colors

- Building materials should be of high quality and durability.
- New commercial and industrial buildings should be constructed primarily of traditional masonry building materials such as brick or stone. These materials should be used on all sides of the building. Recommended accent materials include stone, simulated stone, terracotta, and wood and metal trim.
- While “exterior insulation finish systems” (EIFS) might be used in limited quantities as an accent material, they should not be employed as a primary building material. Rough sawed wood, aluminum siding, rustic shingles and shakes, and plastic or metal panels should be discouraged within the Corridor.
- Color should be used to unite the elements of a façade and to highlight architectural features. However, the colors on individual buildings should complement and be compatible with the colors of nearby buildings.



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- The predominant colors for new buildings should be relatively muted and subtle. While the natural brick and stone colors of red, buff, cream, and gray should predominate, contrasting and complementary colors should also be used to accent building components, highlight architectural elements and to help distinguish the Orchard Road/Blackberry Creek Corridor.

Lighting

- Lighting should be used to illuminate entries, signage, displays, and pedestrian and parking areas, as well as to highlight significant architectural elements.
- Front and rear building entries should be adequately lit for overall security and visibility.
- Building lighting should be subtle and understated; light fixtures should be designed and oriented to produce minimal glare and spill-over onto nearby properties.
- Down-lighting should be encouraged on all commercial buildings; spot-lighting should be limited to major features and key landmarks within the Corridor, including gateway entry features.
- Incandescent lighting creates a warm atmosphere and should be encouraged; if neon lighting is used, colors should be compatible with and complement the façade of the building. High pressure sodium (orange lighting), or a diffused, soft white light should be encouraged; metal halide lighting should be

avoided. Extensive lighting should also be avoided to reduce light pollution.

- Most exterior lighting should be concealed to provide indirect illumination. Where concealment is not practical, light fixtures should be compatible with building architecture.
- Lighting within industrial sites for security and night-time operation should be constructed as necessary. Overspill lighting into adjacent properties should be avoided.
- Parking lots within the commercial and business park areas should consider the pedestrian scale of the Corridor. This can be accomplished by using shorter lighting standards (12'-18') to help establish pedestrian environments.

Parking Lots

- The number of curb cuts along Orchard Road should be minimized. On the east side of Orchard Road, within the Industrial and Business Park Uses, cross access between service roads should be encouraged. Along the west side, within the Commercial areas, parking lots should be connected and access points should be unified. All parking areas should be accessible from cross streets whenever possible.
- All parking areas should be paved or bricked, striped and have surfaces in good condition. The use of pervious brick paver systems should be encouraged.



- Parking lots should be screened from view along sidewalks and roadways through the combined use of free forming berms, low masonry walls, hedge plantings, and shrubs.
- Parking lots should have curbed perimeters and curbed landscaped islands. Clearly marked pedestrian pathways should be provided within the interior of parking areas to avoid large expanses of asphalt and to enhance pedestrian safety.
- Parking lot landscaping should include “landscape islands” at a ratio of approximately one per fifteen to twenty parking spaces. Landscape islands should consist of canopy trees (3 to 4 inches in caliper), attractive groundcover, and/or decorative bushes (2 to 3 feet in height).
- All parking lots should be designed for proper drainage.
- Parking lots that are used during evening hours should be adequately illuminated.
- Whenever possible, parking lots should be shared between multiple stores and businesses to allow for a more efficient lot layout and to minimize curb cuts. Cross access between adjacent parking lots should also be encouraged.

Landscaping and Site Improvements

- Native plants to the Fox River Valley and Northeastern Illinois Region should be encouraged for all landscaped areas within the Corridor. Although initially these plants and grasses may be difficult to establish, long term maintenance costs will be reduced.
- Planters and landscaped areas should buffer parking and service areas. Plantings should consist of low evergreen and/or deciduous shrubs planted in conjunction with low-growing annual or perennial plants and groundcover. Large expanses of exposed mulch should be avoided.
- A landscaped buffer at least five to eight feet in width should be provided around the perimeter of surface parking lots. Perimeter landscape plantings should include a continuous hedge (3’-4’ in height) accented by trees and ground cover.
- In conjunction with landscaping, decorative fencing should be encouraged to delineate and screen parking and service facilities, outdoor storage areas, etc. Low-profile, decorative wrought iron fencing or masonry walls are recommended; chain link fencing should not be permitted within the Corridor.
- All landscaping must be maintained in a healthy and attractive condition. Maintenance programs should be established as part of new development approvals to ensure that private landscaping is adequately cared for and that its value is retained over time. Regular maintenance should include turf mowing, periodic fertilization, pruning, and the cleanup of litter and debris. Internal irrigation systems are preferred and should be encouraged.
- Where site size or configuration does not allow for the minimum five-foot perimeter landscaping, vertical landscaping elements should be employed to provide an effective



screen between parking lots and the public right-of-way.

- Chain link and stockade fencing should be prohibited along Orchard Road. If industrial sites within the Industrial or Business Park areas desire fencing for security purposes, decorative metal fencing should be installed. If chain link fence is absolutely necessary, black-coated vinyl fencing is most appropriate.
- Areas adjacent to entrances, monument signs and other site features should be considered for seasonal flowers or colorful groundcover.
- Evergreens, berming, and other “vertical” landscaping elements should be used to screen industrial and intense commercial activity from adjacent residential properties. Masonry walls may be considered as a screening device where landscaping alone is incapable of providing an effective screen. A non-contiguous berm could screen industrial uses and at the same time provide attractive views into the industrial development. Berming may be most appropriate between residential development and Orchard Road.
- A Streetscape Improvement Program should be implemented to “visually unify” the corridor. The Village of Montgomery should develop and implement a detailed Streetscape Improvement Plan for the entire corridor to establish a visual identity for the street, and to link together the diverse land uses. Streetscape improvements to the corridor should include coordinated landscaping on both sides of the street; pedestrian lighting; distinctive

signage and banners; coordinated street furniture including benches, bike racks and trash receptacles; accent landscaping at key locations; and gateway signage.

- Regularly spaced street trees should be planted in rows along both sides of all streets within the Corridor. Species and spacing should conform to the landscape requirements in the Village of Montgomery’s Landscaping requirements.
- Parkway landscaping should consist of salt-tolerant street trees, shrubs, groundcover and perennials. Plantings in raised beds, planters, urns, or other containers should be considered along the curb line in selected locations and to highlight key entrances and activity areas. Plantings need to be attractively maintained throughout all seasons. Consideration should also be given to an appropriate mix of plant materials to ensure screening and greening still occurs during winter months.
- All landscaping within and adjacent to the public right-of-way should be compatible with existing plant materials in the area and be composed of native and salt tolerant species.
- Street trees and other landscaping along the public rights-of-way should be protected from motorized and pedestrian traffic by curbs, tree grates, and other devices.

Commercial, Industrial, and Business Park

In addition to the general guidelines applicable to all sites and buildings within the Orchard Road/Blackberry Creek Corridor, there are several additional guidelines specifically related to commercial, industrial and business park properties.

These guidelines for commercial, industrial, and business park developments should be used to evaluate and guide new proposals within the Corridor.

- Although the corridor is primarily oriented to the automobile, commercial buildings should have a strong pedestrian orientation with display windows, attractive detailing, and convenient and “hospitable” entrances.
- Covered walkways, such as arcades, should be encouraged along the fronts of multi-tenant commercial buildings to create a “pedestrian-friendly” orientation. The columns of the arcade should be aligned with the façade of the building.
- When commercial buildings are adjacent to, or within close proximity of, Blackberry Creek, or a recreational trail or walkway, the buildings should have attractive rear façades that are comparable to front façades. In essence, the buildings should have two “fronts,” treating the Blackberry Creek Corridor entrances the same as a street entrance.
- All service entrances, dumpsters, loading facilities, and outdoor storage should be located at the rear of buildings; they should be screened from view along sidewalks and roadways through the use of decorative screening, masonry walls, and/or evergreen plantings.
- Any business permitted to have a “drive-thru” facility should be sited so that drive-through lanes and pickup windows are not prominently featured.
- Window glazing should be clear or slightly tinted; dark, mirrored, or reflective glass should not be permitted.
- Any outdoor seating areas, such as those provided by restaurants or cafés, should be well landscaped and incorporated into the overall site design. Outdoor seating areas should be set back and screened from parking areas and driving aisles.
- Awnings and canopies could be used to provide weather protection and to add visual interest at the street level. They should be integrated into the façade and should be in character with the architectural style of the building. Arched or rounded awnings should be discouraged, unless they are compatible with and appropriate to the architectural style of a building. Internally illuminated or back lit awnings and canopies, shingle and mansard canopies, and plastic awnings should not be permitted.

Business Signage

- Exterior building signs should be limited to business identification and description; exterior advertising signs should not be permitted. The size, material, color, and shape of building signs should complement the architectural style and scale of the building.
- Wall-mounted signs should be encouraged, although signage should not project above the cornice line or be mounted on the roof of a building.



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- Raised, individual letters mounted directly on the building, as well as signs that use light colors for lettering and darker colors for backgrounds, should be encouraged.
- When a building contains multiple ground-floor tenants, signage for all businesses should be compatible in design and consistent in placement.
- Street numbers should be prominently displayed at the main entrance to every home or business and be clearly visible from the street.
- Free-standing signage within the Corridor should be limited to low-profile monument signs for multi-tenant commercial properties. Monument signs should be attractively landscaped and constructed of traditional building materials similar to the primary building on the site.
- The letters of a monument sign should be internally illuminated with a white light source. External illumination may be appropriate for a natural metal or engraved stone monument sign.
- Provide "gateway" signage and design features at key intersections along Orchard Road, such as the intersection of Jericho Road, Aucutt Road, US. Route 30, and Galena Road. "Gateway" treatments may include special landscaping, public art, and/or lighting, which would distinguish the Orchard Road Corridor as a unique and identifiable business area.
- The size of signs should complement façade proportions, and should be constructed from the appropriate scale of the building.
- The design and color of the sign should compliment the architectural style of the building, and integrated as a component of the building's facade.
- Internal illumination is preferred for building mounted signs. Externally lighted signs may also be acceptable if they are compatible with other architectural components.
- Pole signs, pylon signs, and billboards should not be permitted within the Orchard Road/Blackberry Creek Corridor.

Residential Areas

In addition to the general guidelines applicable to all sites and buildings within the Orchard Road/Blackberry Creek Corridor, there are several additional guidelines specifically related to residential properties.

- New housing construction should complement the existing homes and neighborhoods, in terms of building materials, colors, and mass and proportion.
- Residential development should align streets so that homes do not “front” Orchard Road or other collectors. A landscaping buffer, including berms should be used to screen the rear of the residential properties from the roadways.
- Clustered residential development should be used to preserve the natural environment. Clustering of housing allows for more open space than traditional development. The additional open space should be used to protect significant natural resources within the corridor.
- When residential development is adjacent to commercial or other incompatible land uses, larger setbacks should be used to provide adequate protection from any adverse impacts.
- Conservation design and development concepts should be encouraged for all residential development. Porous brick driveways, naturalized drainage swales bioswales, and narrower roadways are examples of some techniques that could be promoted within the Corridor.

Public Improvements

In addition to site and building improvements on industrial/business park, commercial and residential sites, a range of projects should be undertaken within the public rights-of-way and on publicly owned property to enhance the image and appearance of the Orchard Road/Blackberry Creek Corridor to create a safe, attractive, and “hospitable” working, shopping, living, and leisure-time environment. These public sector improvements can also help promote new private investment and development, and attract additional visitors and business patrons to the area.

In general, it is recommended that the Village establish a comprehensive, area-wide design system for public improvements to be applied in various parts of the corridor. While the design system should be based on the architecture, history, and traditional character of Montgomery and the Fox River Valley region, this system should also help establish a unique new image and identity for the Orchard Road/Blackberry Creek Corridor.

The guidelines presented in this section provide a preliminary framework for the design treatment of streets, sidewalks, streetscape facilities, and other pedestrian and open space amenities.

Streets

- Even though safe and convenient vehicular access and circulation will be essential, the Corridor should be improved and redeveloped in a manner that encourages alternative modes of transportation, including walking, and bicycling, in order to reduce the reliance on automobile travel, and to create a corridor that is more livable and pedestrian friendly.
- Streets should be designed to support vehicles, bicyclists and pedestrians on a relatively equal basis. While accommodating vehicular traffic, streets should also promote walking, cycling, and an overall sense of place.
- The use of a landscaped median, to create a boulevard along the entire length of Orchard Road should be considered. The landscape median will enhance the appearance of the roadway, calm traffic, and distinguish the Corridor from adjacent communities.
- Streets within the Corridor should be designed and controlled to reduce the speed of traffic as it passes through Montgomery, in order to protect pedestrians and enhance commercial activity.
- Street configuration should protect the quiet neighborhoods of the Corridor. Traffic from industrial and commercial uses should be prohibited from entering residential areas. Cut-through traffic should be discouraged through the layout and design of the residential development.
- Street crossings should be improved at the intersection of Orchard Road and Jericho

Road, Aucutt Road, and Galena, and U.S. Route 30 and Griffin Drive. Improved crossings would make the Corridor safer and more comfortable for pedestrians to access all areas within the Corridor, including shopping areas, employment areas and parks and open space. Improving the street crossings may entail widening sidewalks, constructing curb extensions, improved pavement markings, pedestrian-compatible traffic signals, and other measures.

- To improve visibility and safety, crosswalks should be made prominent and noticeable by employing a change in paving materials, texture and color. Small pylons and special bollard lighting fixtures might also be used to highlight crosswalks.

Sidewalks

- Sidewalks should be designed for pedestrians. All public sidewalks should be a minimum of six feet in width, with eight feet being considered ideal.
- Along Orchard Road, a “buffer” zone between the walkway and the street should be constructed. This buffer zone should consist of a grass parkway with street trees and light fixtures, or a paved area with trees in grates, lights, and other street furniture.
- Sidewalks should be provided along both sides of all streets within the Corridor.



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- An area-wide system of secondary walkways should also be developed within commercial development areas, to provide convenient linkages between public sidewalks and nearby parking areas, open spaces and building entries.
- All public and private sidewalks within the Corridor should be accessible to the handicapped and should comply with appropriate ADA (Americans With Disabilities Act) standards.

Lighting

- Lighting along public streets within the Corridor should consist of both roadway lighting and decorative pedestrian lighting.
- Pedestrian light fixtures should be approximately 12 to 15 feet, and be of a style and character that would complement existing and proposed development.
- Pylons and bollard lighting should be considered as accents and for ornamental purposes. These fixtures could be used to highlight crosswalks, open spaces, seating areas, and major pedestrian ways.

Public Signage

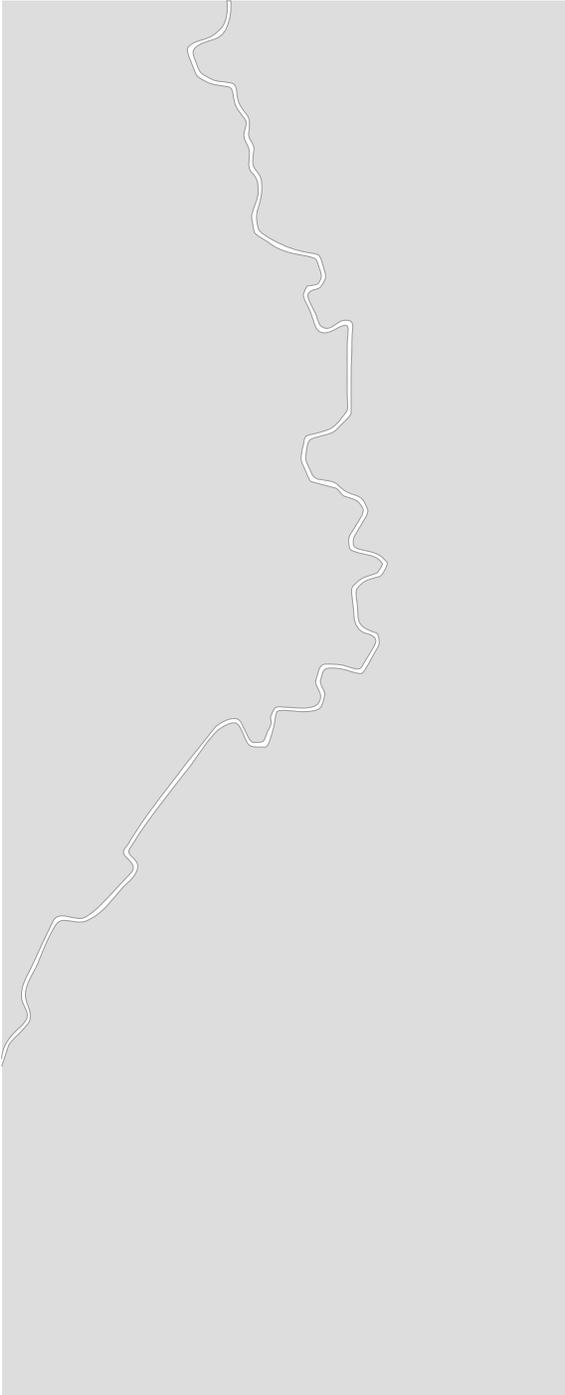
- The Village should establish a comprehensive signage system that can direct motorists to the Corridor, and guide and direct both motorists and pedestrians to key destinations within the area.
- “Gateway” signs should be developed along Orchard Road, as it serves as one of the primary entry ways into the Village of Mont-

gomery. In addition to signage, gateway features might include special landscaping, lighting and paving.

- Banners attached to street light standards will distinguish the Corridor and could also be used to commemorate special events within the Village of Montgomery and the surrounding area. Banners might be changed periodically during the year.
- A new “icon” should be considered to specifically brand the Orchard Road/Blackberry Creek Corridor. This new icon could appear on the full range of Corridor signs and directories, and also be used in conjunction with the Village’s current logo in a variety of marketing and promotional efforts.

Park, Open Space, Recreation and Pedestrian Amenities

- A greenway along Blackberry Creek should be developed as part of Corridor’s development. The greenway will serve as a focal point for new development, become an attraction for shoppers, residents, visitors, and employees, and be a recreational and environmental amenity to the community. A 10’ wide asphalt trail should be constructed along the greenway.
- Improvement and development of Orchard Road should include a unified system of “street furnishings,” such as seating areas, trash receptacles, drinking fountains, bike racks, and other pedestrian amenities. Street furnishings should be consistent with the materials, colors and architectural styles to be promoted within the Corridor.



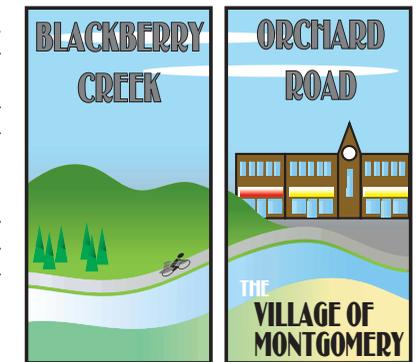
- Trash receptacles should be placed along Orchard Road, and within commercial areas. Benches and drinking fountains should be located at trailheads along the Blackberry Creek Greenway and high activity areas. Bike parking should be provided in visible areas and near main entrances to buildings. Bollards should be used at curb side, along streets where sidewalks directly abut parking or traffic lanes.

Public Utilities

- Whenever possible, utility lines within the Corridor should be placed underground.

Figure 5 Design & Development Examples

Design and development guidelines should steer both public and private improvements towards the desired character and appearance for the Orchard Road/Blackberry Creek Corridor. The guidelines should not attempt to dictate architectural style, or try to make all buildings look the same. Rather they strive to promote high quality, compatible, and attractive development.



An example of how a street banner that promotes shopping and environmental protection and enhancement could look along Orchard Road.



1. Parking lot landscaping should include "depressed landscape islands" planted with native grasses and wildflowers which collect rain water and help reduce run-off. The landscaping also reduces light glare, noise, dust, and reduces the monotony, and softens the visual impact of parking areas.



5. Signs on buildings should consist of individually projected letters mounted flat on the building face.



2. Evergreens, berming and other vertical landscaping elements should be used to screen industrial and intense commercial activity from adjacent residential properties.



6. Planters and landscaped areas should be encouraged adjacent to commercial and industrial buildings within the Corridor. Foundation landscaping should consist of trees, shrubs, and seasonal flower beds.



3. A full range of streetscape improvements, including attractive and coordinated landscaping, buffering, lighting, distinctive signage and banners, street furniture, and gateway signs will improve the appearance of the Corridor and distinguish it from adjacent communities.



7. Sidewalks should be provided on both sides of Orchard Road along its entire length. Sidewalks should be wide enough to accommodate bicycle activity. Attractive right-of-way landscaping can protect the pedestrian, screen unsightly uses and visually unify the Corridor.



4. Attractive, low profile monument signs should be encouraged for commercial, industrial and business park uses. Monument signs should be attractively landscaped and constructed from traditional building materials similar to the primary building.



8. Buildings should "front" Orchard Road wherever possible. Long, blank facades along Orchard Road, or other public streets should be avoided. Buildings should have either a pitched or flat roof.



9. The recreational trail along Blackberry Creek should provide linkages to commercial and residential areas and contain pedestrian amenities, including seating areas, drinking fountains, and trash receptacles.



10. "Street furnishings" significantly add to an area's pedestrian appeal. Benches, trash receptacles, drinking fountains, and bike racks help create a pedestrian oriented area.



11. Lighting serves several purposes, including advertising a business, discouraging crime, and improving building appearance. Indirect illumination and building mounted lighting should be used to improve the visual impact of buildings within the Corridor.



12. A pedestrian arcade can add pedestrian scale to a large commercial building. Vertical architectural elements can also provide interest and help unify and distinguish the corridor.

ORCHARD ROAD CROSS SECTION - Graphic Representation - Not To Scale

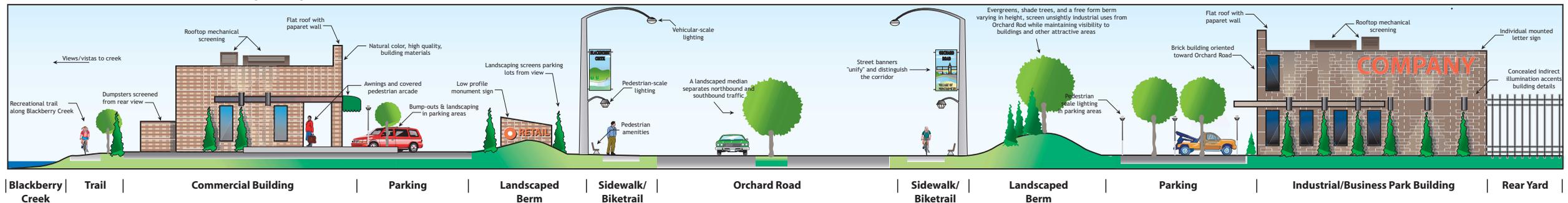
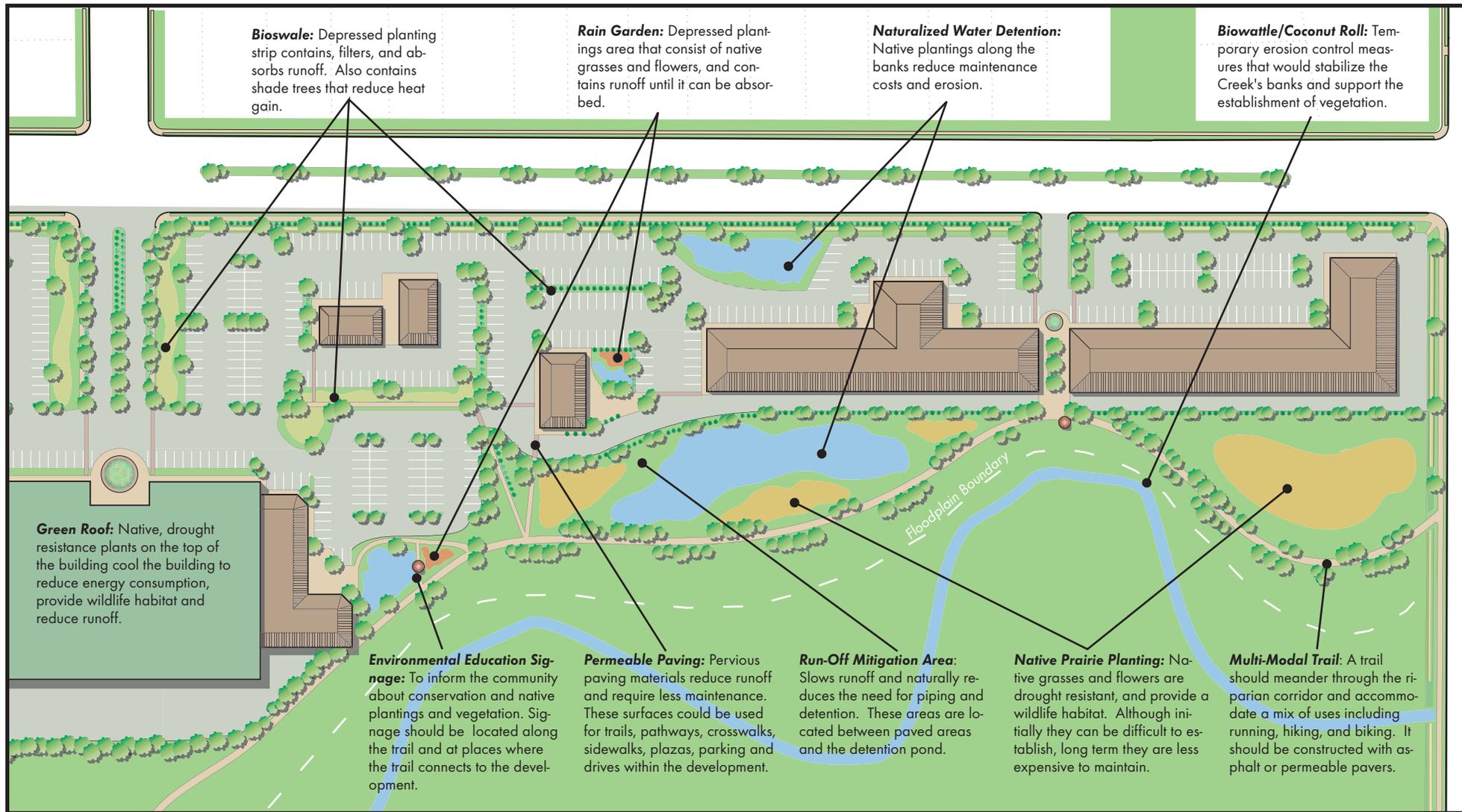


Figure 6
Illustrative Concept



Implementation

The Orchard Road/Blackberry Creek Corridor Plan sets forth an agreed-upon “road map” for growth and development within the Village of Montgomery, and more specifically the Corridor’s planning boundary, during the next ten to fifteen year period. It is the product of considerable effort on the part of the Village Board, the Planning Advisory Committee, Village staff, the Plan Commission, and the Montgomery community. However, in many ways the planning process for the Orchard Road/Blackberry Creek Corridor has just begun. Completion of this Corridor Plan is only the first step, not the last.

This section briefly highlights several next steps that should be undertaken to begin the process of plan implementation. These include:

- Adopt and use the Orchard Road/Blackberry Creek Corridor Plan on a day-to-day basis;
- Review and update the Zoning Ordinance and other development controls;
- Develop and utilize a regular Capital Improvements Program (CIP);
- Promote cooperation and participation among various agencies, organizations, community groups and individuals;
- Prepare an implementation “action agenda;”
- Explore possible funding sources and implementation techniques;
- Enhance public communication; and
- Update the Orchard Road/Blackberry Creek Corridor Plan on a regular basis.

Adopt and Use the Plan on a Day-to-Day Basis

The Orchard Road/Blackberry Creek Corridor Plan should become Montgomery’s official policy guide for improvement and development within the Corridor. It is essential that the Plan be adopted by the Village Board and then be used on a regular basis by Village staff, boards and commissions to review and evaluate all proposals for improvement and development within the study area in the years ahead.

Review the Zoning Ordinance and Other Development Codes

Zoning is an important tool in implementing planning policy. It establishes the types of uses to be allowed on specific properties, and prescribes the overall character and intensity of development to be permitted.

Adoption of the new Plan should be followed by a review and update of the Village’s various development controls including zoning, subdivision regulations, and other related codes and ordinances. It is essential that all development controls be consistent with, and complement, the Orchard Road/Blackberry Creek Corridor Plan.

The Corridor Plan sets forth policies regarding the use of land within the study area and establishes guidelines for the quality, character and intensity of new development to be promoted in the years ahead. The Plan’s policies and guidelines should greatly assist the Village in formulating new zoning and development code regulations that can better reflect the unique needs and aspirations of the Montgomery community, particularly within the corridor’s boundaries.

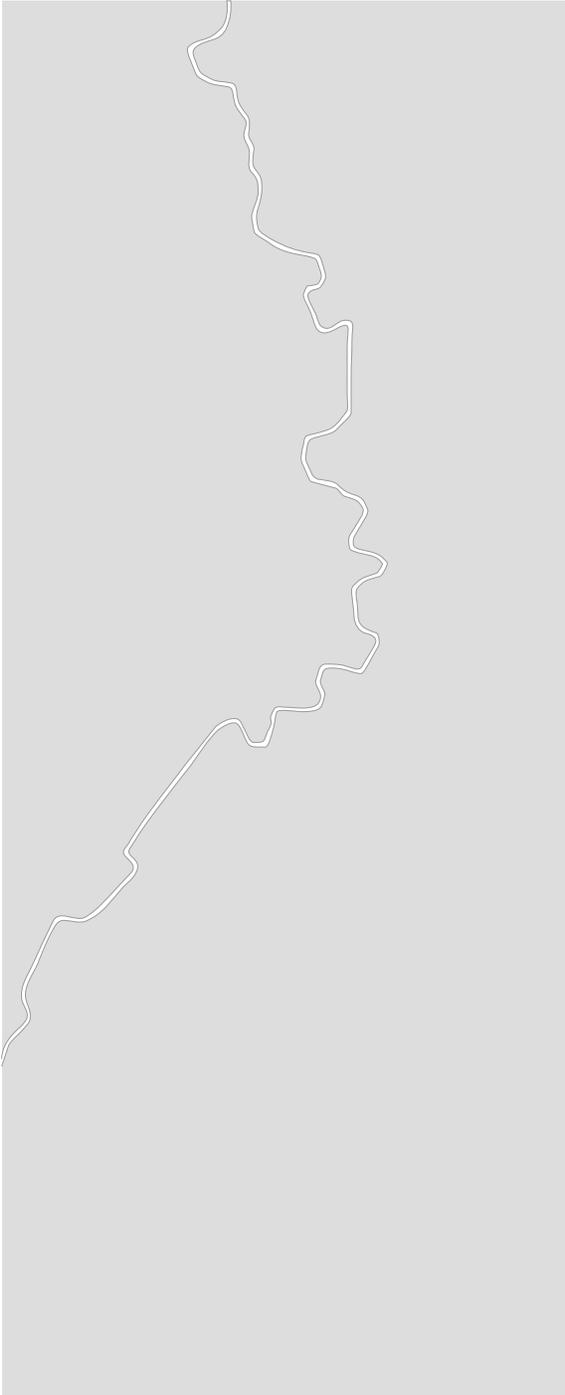
Capital Improvements Program

Another tool for implementing the Plan is the Capital Improvements Program (CIP). Capital improvements identified in the Corridor Plan should be included in the Village of Montgomery’s Capital Improvements Program.

The CIP establishes schedules and priorities for all public improvement projects within a five-year period. The Village first prepares a list of all public improvements that will be required in the next five years. Then all projects are reviewed, priorities assigned, cost estimates prepared, and potential funding sources identified.

The Capital Improvements Program typically schedules the implementation of a range of specific projects related to the Orchard Road and Blackberry Creek Corridor Plan, particularly the restoration and upgrading of existing utilities and infrastructure facilities, including the water system, sanitary sewers, storm water facilities, and the street system. Development or improvement of Civic Center Campus, fire station, telecommunications infrastructure, and public works facilities would also be included in the Capital Improvements Program.

Montgomery’s financial resources will always be limited and public dollars must be spent wisely. The Capital Improvements Program would allow the Village of Montgomery to provide the most desirable public improvements, yet stay within budget constraints.



Village of Montgomery: Orchard Road/Blackberry Creek Corridor Plan

Promote Cooperation and Participation

The Village of Montgomery should assume the leadership role in implementing the Corridor Plan. In addition to carrying out the administrative actions and many of the public improvement projects called for in the Plan, the Village may choose to administer a variety of programs available to local residents, businesses and property owners.

In order for the Orchard Road/Blackberry Creek Corridor Plan to be successful it must be based on a strong partnership between the Village, other public agencies, various neighborhood groups and organizations, the local business community, and the private sector.

The Village should be the leader in promoting the cooperation and collaboration needed to implement the new Corridor Plan. The Village's "partners" should include:

- Other governmental and service districts, such as the school districts, the fire protection districts, the Fox Valley Park District, the Townships, Kane and Kendall Counties, the private utility companies, the Illinois Department of Transportation (IDOT), the Conservation Foundation, the Kane and Kendall County Forest Preserve Districts, etc;
- Local institutions, such as the Greater Montgomery Area Chamber of Commerce and various churches and religious organizations;
- Local banks and financial institutions, which can provide assistance in upgrading existing

properties and facilitating desirable new development;

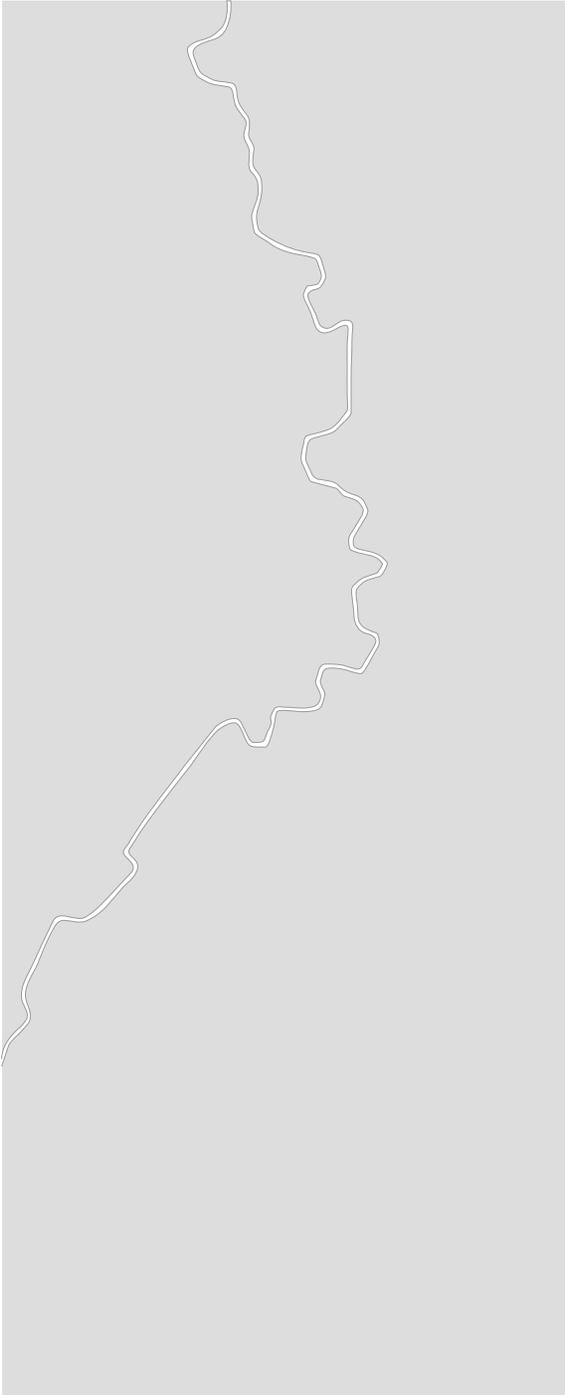
- Builders and developers, who should be encouraged to undertake improvements and new construction that conform to the Plan and enhance the overall quality and character of the Orchard Road/Blackberry Creek; and
- The Montgomery community, since all residents and neighborhood groups should be encouraged to participate in the on-going planning process, and all should be given the opportunity to voice their opinions on improvement and development decisions within the community.

Prepare an Implementation Action Agenda

The Village should prepare an implementation "action agenda" which highlights the improvement and development projects and activities to be undertaken during the next few years. For example, the "action agenda" might consist of:

- A detailed description of the projects and activities to be undertaken;
- The priority of each project or activity;
- An indication of the public and private sector responsibilities for initiating and participating in each activity; and
- A suggestion of the funding sources and assistance programs that might potentially be available for implementing each project or activity.

In order to remain current, the "action agenda" should be updated once a year.



Village of Montgomery: Orchard Road/Blackberry Creek Corridor Plan

Explore Funding Sources and Implementation Techniques

While many of the projects and improvements called for in the Corridor Plan can be implemented through administrative and policy decisions or can be funded through normal municipal programs, other projects may require special technical and/or financial assistance.

The Village should continue to explore and consider the wide range of local, state and federal resources and programs that may be available to assist in the implementation of planning recommendations.

Enhance Public Communication

The Village should prepare a brief summary version of the Orchard Road/Blackberry Creek Corridor Plan and should make the plan available to the community, particularly to property owners within the Corridor. It is important that all local residents, businesses and property owners be familiar with the Plan's major recommendations and its vision for the future.

The Village should also consider additional techniques for responding quickly to public questions and concerns regarding planning and development. For example, the Village might prepare a new informational brochure on how to apply for zoning, building, subdivision and other development-oriented permits and approvals. It might also consider special newsletter or Web page features that focus on frequently raised questions and concerns regarding planning and development.

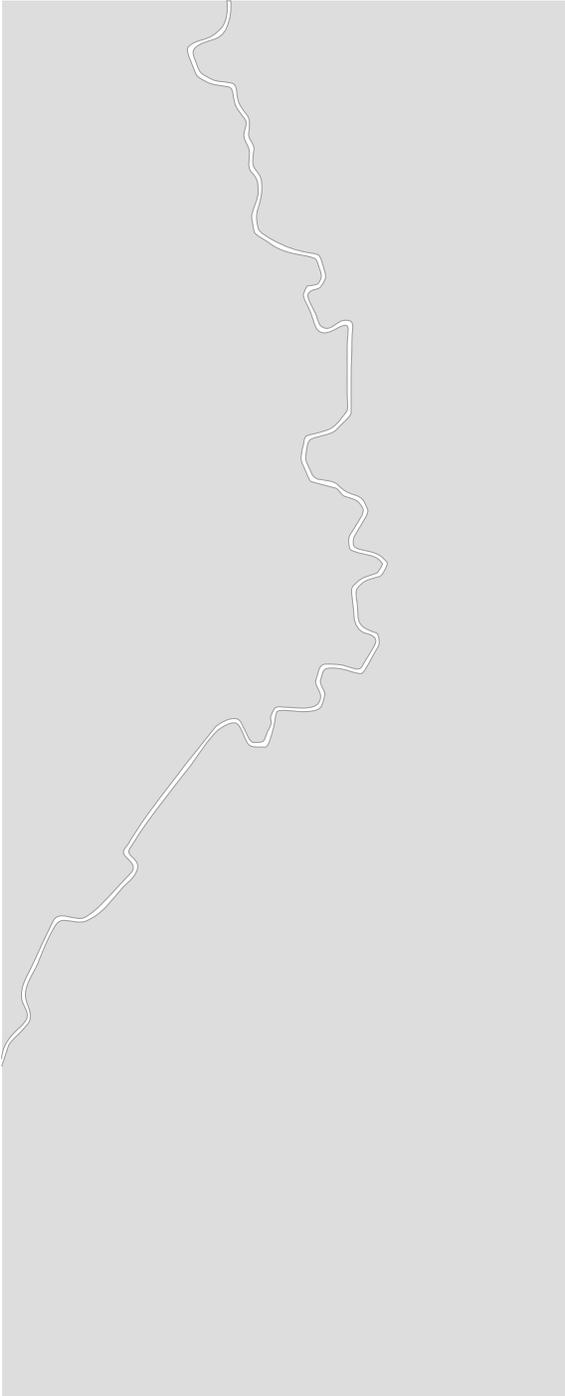
Update the Plan on a Regular Basis

It is important to emphasize that the Plan is not a static document. If community attitudes change or new issues arise which are beyond the scope of the current Plan, the Plan should be revised and updated accordingly.

Although a proposal to amend the Plan can be brought forth by petition at any time, the Village should regularly undertake a systematic review of the Plan. Although an annual review is desirable, the Village should initiate review of the Plan at least every two to three years. Ideally, this review should coincide with the preparation of the annual budget, the capital improvement program, and the Comprehensive Plan review. In this manner, recommendations or changes relating to capital improvements or other programs can be considered as part of the upcoming commitments for the fiscal year. Routine examination of the Plan will help ensure that the planning program remains relevant to community needs and aspirations.

The Village should:

- Make available copies of the Plan document for public purchase.
- Make an electronic version of the Plan available for download from the Village's website.
- Provide assistance to the public in explaining the Plan and its relationship to private and public development projects and other proposals, as appropriate.
- Assist the Village Board in the day-to-day administration, interpretation and application of the Plan.



- Maintain a list of current possible amendments, issues or needs which may be a subject of change, addition or deletion from the Plan.
- Coordinate with, and assist the Plan Commission and Village Board in the Plan amendment process.